

# CITY of DALLAS

## Standard Drawings

### & Oregon Standard Drawing Amendments

#### Drainage Drawings

- RD300 (A) - Standard Trench Details
- RD302 (A) - Trench Repair
- RD303 - Trench Repair on New or Resurfaced Street
- RD310 (A) - Sanitary and Storm Sewer Lateral Detail
- RD335 (A) - Standard Storm Sewer Manhole
- RD342 (A) - Shallow Manholes
- RD350 (A) - Standard Drop Manhole Detail
- RD356 (A) - Manhole Covers and Frames
- RD360 (A) - Standard Manhole Adjustment in Streets
- RD362 (A) - Standard Sanitary-Storm Cleanout Detail
- RD366 (A) - Standard Catch Basin Detail (CG-2)
- RD370 (A) - Field Inlet (Type D)
- RD371 (A) - Standard Curb Inlet and Base Detail (CG-3)



April 2016



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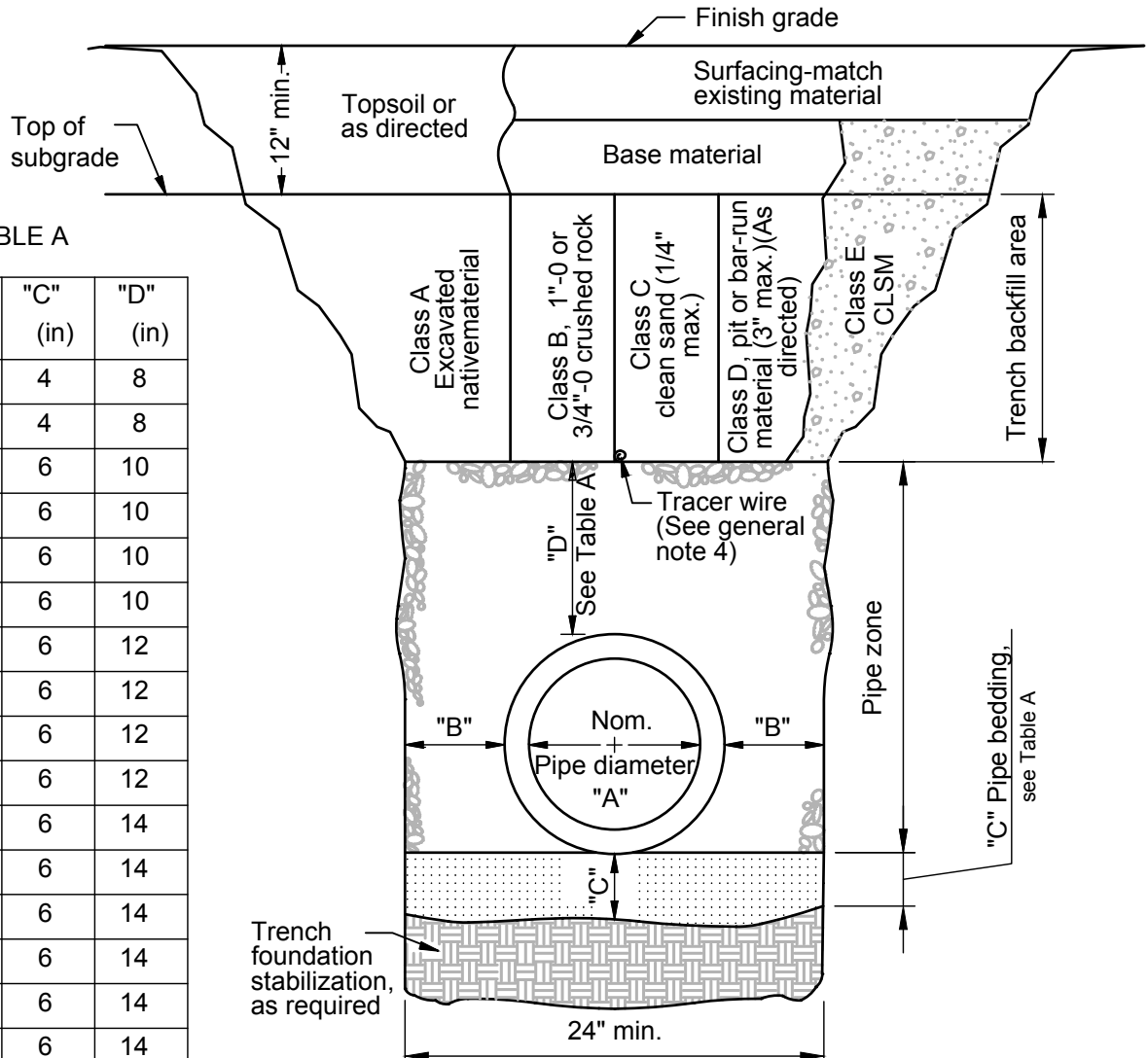


TABLE A

| "A"<br>(in) | "B"<br>(in) | "C"<br>(in) | "D"<br>(in) |
|-------------|-------------|-------------|-------------|
| 4           | 10          | 4           | 8           |
| 6           | 10          | 4           | 8           |
| 8           | 10          | 6           | 10          |
| 10          | 10          | 6           | 10          |
| 12          | 12          | 6           | 10          |
| 15          | 12          | 6           | 10          |
| 18          | 16          | 6           | 12          |
| 21          | 16          | 6           | 12          |
| 24          | 18          | 6           | 12          |
| 30          | 18          | 6           | 12          |
| 36          | 24          | 6           | 14          |
| 42          | 24          | 6           | 14          |
| 48          | 24          | 6           | 14          |
| 54          | 24          | 6           | 14          |
| 60          | 24          | 6           | 14          |
| 66          | 24          | 6           | 14          |
| 72          | 24          | 6           | 14          |

For pipes over 72" diameter, see general note 3.

MULTIPLE INSTALLATIONS

| DIAMETER   | MIN. SPACE BETWEEN PIPES    |
|------------|-----------------------------|
| Up to 48"  | 24"                         |
| 48" to 72" | One half (1/2) dia. of pipe |

**NOTES:**

- BACKFILL MATERIAL SHALL BE COMPACTED IN 12" TO 18" LIFTS, DEPENDING ON COMPACTING TOOLS USED, AND COMPACTED TO A 95% RELATIVE MAXIMUM DENSITY.
- WHEEL ROLLING TRENCHES WITH BACKHOES OR OTHER EQUIPMENT IS NOT CONSIDERED ACCEPTABLE COMPACTING WORKMANSHIP.
- SURFACING OF PAVED AREAS SHALL COMPLY WITH STREET CUT STD. DRG. RD302.
- FOR PIPE INSTALLATION IN EMBANKMENT AREAS WHERE THE TRENCH METHOD WILL NOT BE USED AND THE PIPE IS ≥ 36" DIAMETER, INCREASE DIMENSION "B" TO NOMINAL PIPE DIAMETER.
- PIPES OVER 72" DIAMETER ARE STRUCTURES, AND ARE NOT APPLICABLE TO THIS DRAWING.
- SEE STD. DRG. RD336 FOR TRACER WIRE DETAILS (WHEN REQUIRED).

**MODIFICATIONS TO RD300:**

- DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD300.
- FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD300.

| NO. | REVISIONS                     | BY  | DATE      |
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| 0   | Adoption of Standard Drawings | CTG | 4/11/2016 |

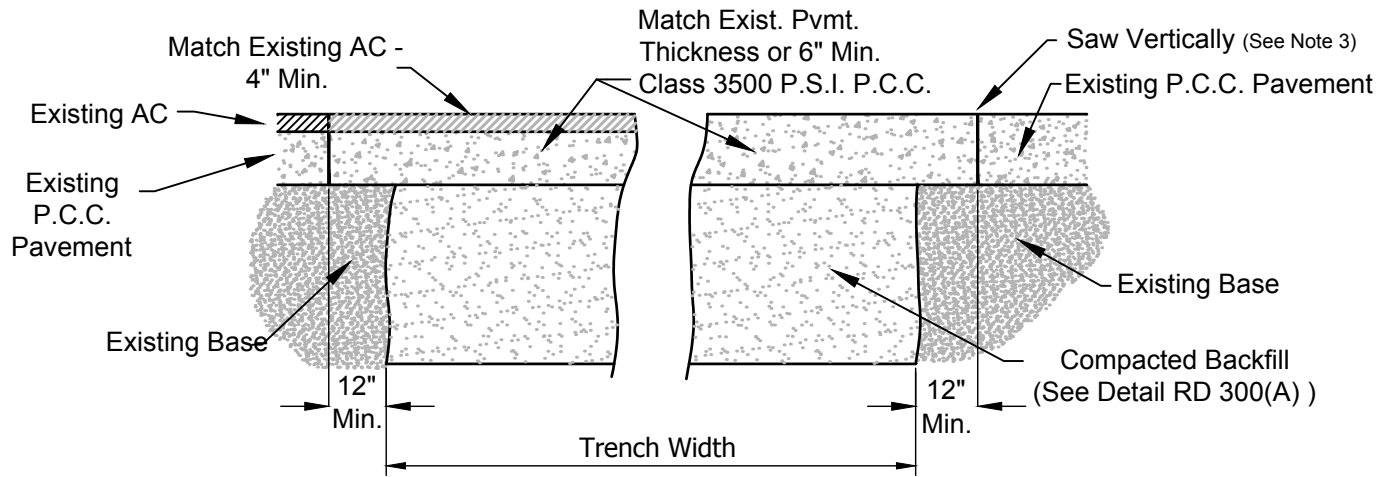


CITY OF DALLAS OREGON  
STANDARD DRAWING AMENDMENT  
STANDARD TRENCH DETAILS

DRAWING NO.  
RD300  
(A)

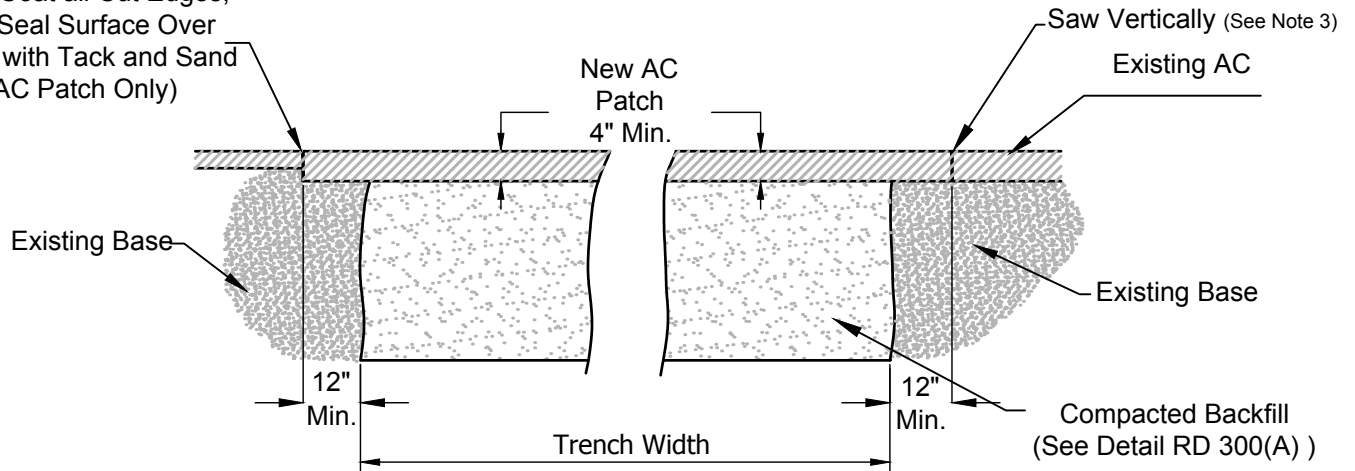
**Half Section**  
**Rigid Pavement w/ AC Surface**

**Half Section**  
**P.C.C. Pavement**



**TYPICAL PATCH FOR RIGID PAVEMENT**

Tack Coat all Cut Edges,  
and Seal Surface Over  
Joints with Tack and Sand  
(AC Patch Only)



**TYPICAL PATCH FOR FLEXIBLE PAVEMENT**

**Notes:**

1. SAWCUT EXISTING ASPHALT A MINIMUM OF 12" TO UNDISTURBED PAVEMENT.
2. SAWCUTS WILL BE MADE PARALLEL TO TRENCH.
3. CUTS IN AC PAVEMENT SHALL BE MADE W/ A CUTTING WHEEL, CONCRETE SAW OR OTHER APPROVED METHOD. CUTS IN P.C.C. PAVEMENT SHALL BE DONE W/ A CONCRETE SAW. INTERMITTENT PUNCHING WITH A POINTED JACK HAMMER SHALL NOT BE PERMITTED.
4. FINAL ASPHALT THICKNESS SHALL BE OF 4", OR AS REQUIRED TO MATCH EXISTING THICKNESS, WHICHEVER IS GREATER.
5. ASPHALT SHALL BE PLACED IN A MINIMUM OF TWO LIFTS. CLASS "C" MIX SHALL BE THE WEARING COURSE.
6. TRENCH SURFACE SHALL BE MAINTAINED DAILY, UNTIL PERMANENT SURFACE REPAIR IS IN PLACE. ON ASPHALT, COLD PATCH MIX SHALL BE USED UNTIL TRENCH IS RESURFACED.
7. IF STREET IS NEW, OR HAS BEEN RESURFACED, USE DETAIL RD303(A).

**MODIFICATIONS TO RD302:**

1. DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD302.
2. FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD302.

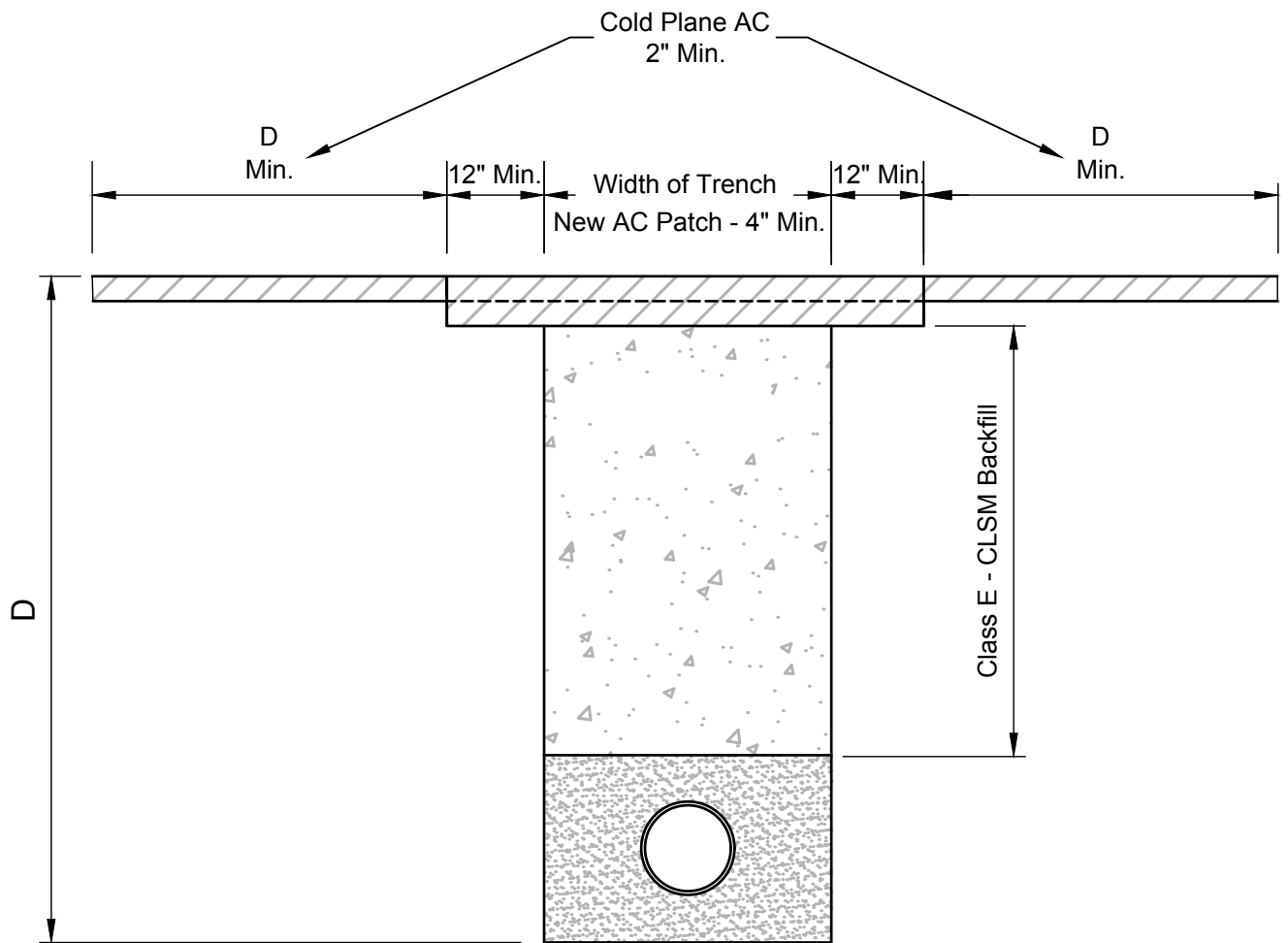


CITY OF DALLAS OREGON  
STANDARD DRAWING AMENDMENT

TRENCH REPAIR

DRAWING NO.  
**RD302**  
**(A)**

|     |                               |     |           |
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| 0   | Adoption of Standard Drawings | CTG | 4/11/2016 |
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**\*\*Tack Coat all Cut Edges,  
and Seal Surface Over  
Joints with Tack and Sand  
(AC Patch Only)**

**NOTES:**

1. SAWCUT EXISTING ASPHALT A MINIMUM OF 12" TO UNDISTURBED PAVEMENT.
2. SAWCUTS WILL BE MADE PARALLEL TO TRENCH.
3. FINAL ASPHALT THICKNESS SHALL BE OF 4", OR AS REQUIRED TO MATCH EXISTING THICKNESS, WHICHEVER IS GREATER.
4. ASPHALT SHALL BE PLACED IN A MINIMUM OF TWO LIFTS. CLASS "C" MIX SHALL BE THE WEARING COURSE.
5. TRENCH SURFACE SHALL BE MAINTAINED DAILY, UNTIL PERMANENT SURFACE REPAIR IS IN PLACE. ON ASPHALT, COLD PATCH MIX SHALL BE USED UNTIL TRENCH IS RESURFACED.
6. COLD PLANE AC MINIMUM OF 2" THE TOTAL WIDTH OF "D".
7. CITY ENGINEER TO DETERMINE IF STREET IS NEW OR RECENTLY RESURFACED.

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**CITY OF DALLAS OREGON  
 STANDARD DRAWING  
 TRENCH REPAIR ON NEW OR RESURFACED  
 STREET**

DRAWING NO.  
**RD303**

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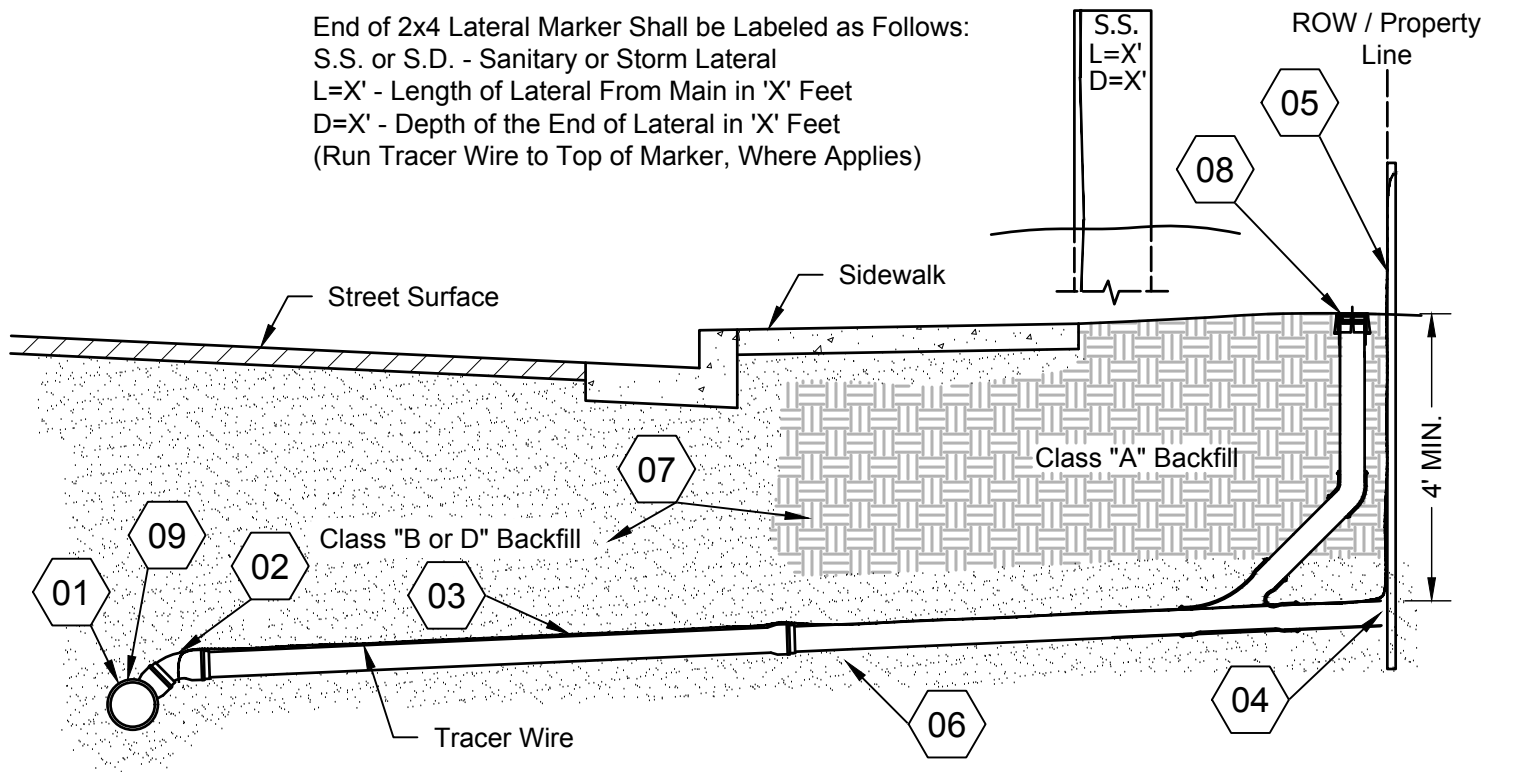


### Legend

- 01. Wye or Tee ASTM D-3034 PVC Fittings, or ASTM C-14, C-76 Concrete Manufactured Fittings.
- 02. 45° or Smaller Bend Required. Insert Into Wye Or Tee.
- 03. Sanitary Laterals are to be ASTM D-3034 PVC GREEN Pipe Only. Storm Laterals are to be ASTM D-3034 PVC WHITE Pipe Only. Laterals shall have nominal inside diameters of 4 and 6 inches and use long radius bends at all changes in direction.
- 04. Mechanical Rubber or Any Other Rubber Sealed, Water Tight Plug.
- 05. A Full Length 2x4 Marker of Which the Top 2 Feet Will be Exposed and Painted Green For SANITARY SEWER and White for STORM SEWER, Marking the End of the Lateral. The Lateral Will Terminate Either at the Property Line or the Lot Side of the PUE (Public Utility Easement), Whichever is Greater.
- 06. Bedding and Pipe Zone: 3/4" Minus Compacted Crushed Rock, Giving the Pipe and Any Fittings a Uniform Firm Bed to Lay on. Pipe Zone Includes a Minimum of 6" of Rock All Around the Pipe.
- 07. Backfill Requirements: Class "B or D" Backfill: Under Pavement and Graveled Street, Driveways and Parking Areas, as Well as Existing Sidewalks and Shoulders. Class "A" Backfill: Under Landscaped Areas, Lawns, Fields and Any Other Area Where Class "B or D" is Not Required. Restore Top 12" to Previous Conditions or Better.
- 08. Install cleanout, see RD362(A).
- 09. No Profinding Taps Allowed.

### NOTES:

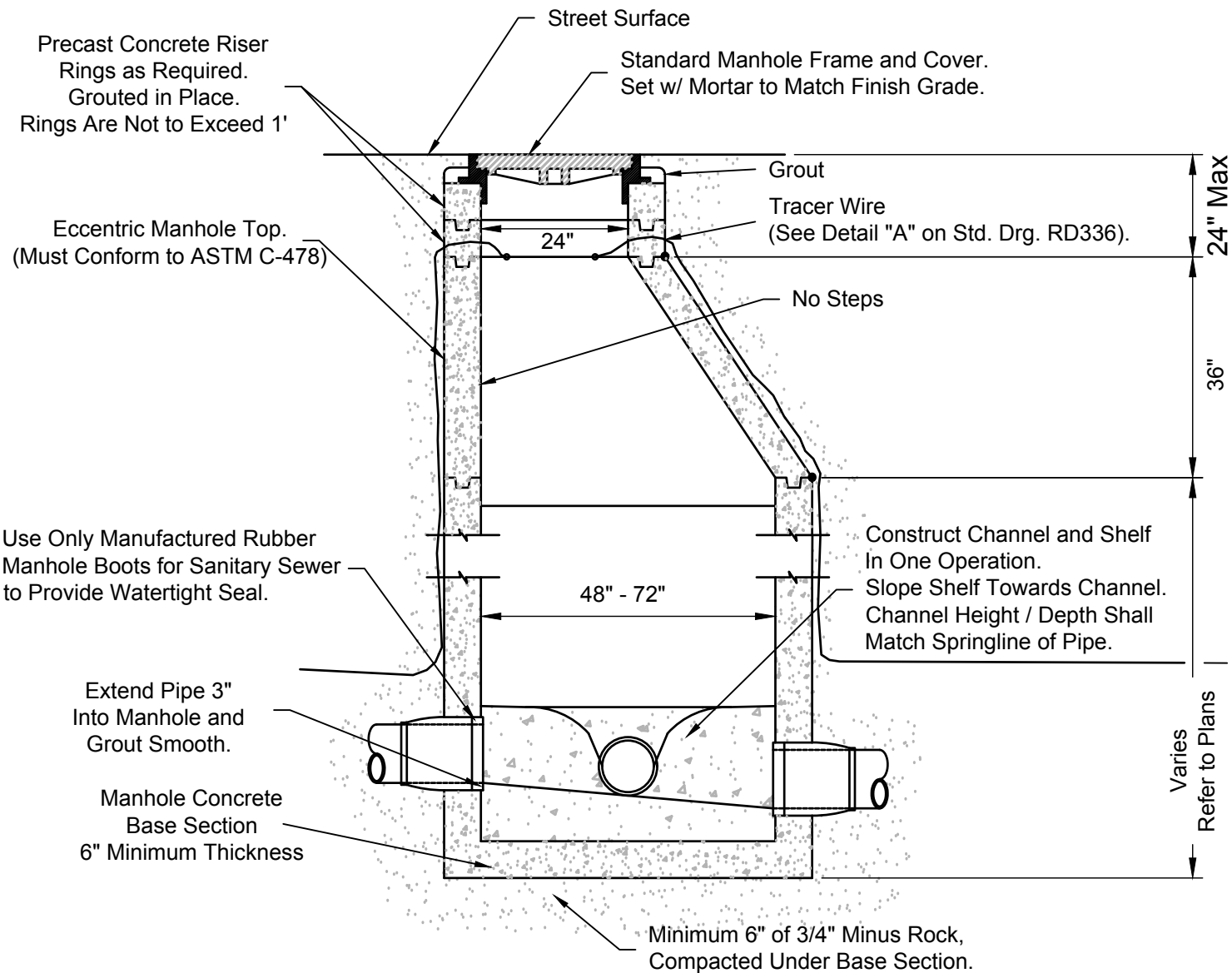
- 1. TAPPING OF EXISTING MAINS SHALL BE DONE BY CITY OF DALLAS PERSONNEL. INVERT AT END OF FITTING IS NOT TO BE LOWER THAN HORIZONTAL CENTER LINE OF MAIN.
- 2. MINIMUM GRADE FOR LATERALS WILL BE 2% OR 1/4" PER FOOT AND MUST BE WATER TIGHT.
- 3. LATERAL WILL DROP INTO MAIN AT TOP HALF OF THE MAIN. ROTATE WYE OR TEE AS NEEDED.
- 4. TRACER WIRE IS REQUIRED ON ALL LATERALS.
- 5. SERVICE CONNECTIONS ARE ALLOWED TO THE SEWER SYSTEM AT A MANHOLE AS APPROVED BY THE CITY ENGINEER.
- 6. WHERE TEES OR WYES FOR CONNECTION ARE ABSENT OR UNUSABLE, CONNECTION OF SERVICE LINES SHALL BE MADE WITH AN APPROVED TAP SUCH AS SCALTITE SADDLE, FOWLER TAP, FOWLER TEE, TAP-TITE TEE, OR EQUAL COMMERCIAL TAP.



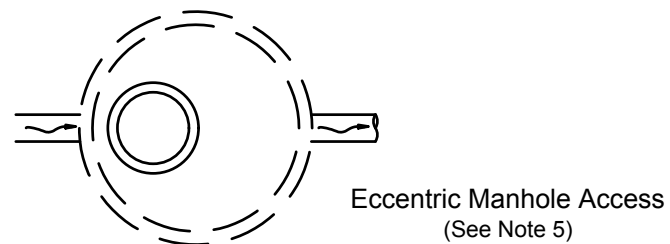
End of 2x4 Lateral Marker Shall be Labeled as Follows:  
 S.S. or S.D. - Sanitary or Storm Lateral  
 L=X' - Length of Lateral From Main in 'X' Feet  
 D=X' - Depth of the End of Lateral in 'X' Feet  
 (Run Tracer Wire to Top of Marker, Where Applies)

|  |                               |             |           |
|--|-------------------------------|-------------|-----------|
| <b>MODIFICATIONS TO RD310:</b>   |                               |             |           |
| 1. DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD310. |                               |             |           |
| 2. FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD310.                       |                               |             |           |
| <b>CITY OF DALLAS OREGON<br/>STANDARD DRAWING AMENDMENT</b>  |                               |             |           |
| <b>SANITARY AND STORM SEWER LATERAL DETAIL</b>   |                               |             |           |
|  |                               | DRAWING NO. |           |
|  |                               | RD310       |           |
|  |                               | (A)         |           |
| 0  | Adoption of Standard Drawings | CTG         | 4/11/2016 |
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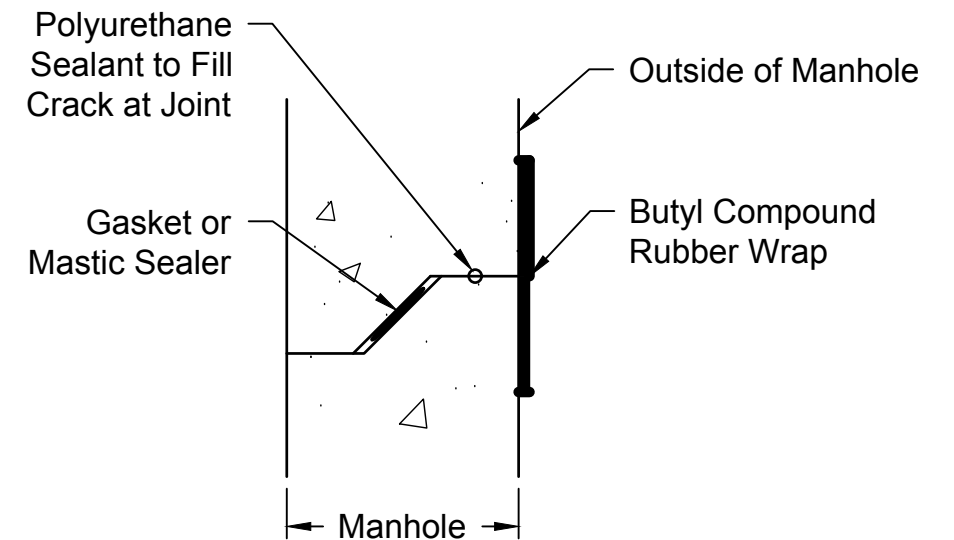




**MANHOLE PROFILE VIEW**



**MANHOLE PLAN VIEW**



**MANHOLE JOINT**

**NOTES:**

- 28-DAY COMPRESSIVE STRENGTH OF CONCRETE SHALL BE A MINIMUM OF 3,500 PSI.
- STANDARD PRECAST MANHOLE SHALL BE A MINIMUM OF 48" DIAMETER.
- ALL CONNECTING PIPES SHALL HAVE A FLEXIBLE JOINT WITHIN 18" OF MANHOLE WALL.
- ALL PRE INSTALLED LADDER ASSEMBLIES SHALL BE PULLED/CUT, AND HOLES GROUTED.
- POSITION ECCENTRIC CONE ACCESS 180° FROM DOWNSTREAM (OUT) FLOW (SEE DETAIL .
- MANHOLE BASE SECTION, BARREL, ECCENTRIC CONE AND GRADE RING SHALL CONSTRUCTED OF PRECAST REINFORCED CONCRETE, CONFORMING TO ASTM C478.
- ALL SANITARY MANHOLES ARE TO PASS A VACUUM OR HYDROSTATIC TEST, SCHEDULED BY CONTRACTOR.
- MANHOLES LESS THAN 4'-0" DEEP SHALL HAVE A PRECAST 8" OSHD FLAT TOP. (SEE DETAIL NO. 342(A) )

**MODIFICATIONS TO RD335:**

- DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD335.
- FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD335.

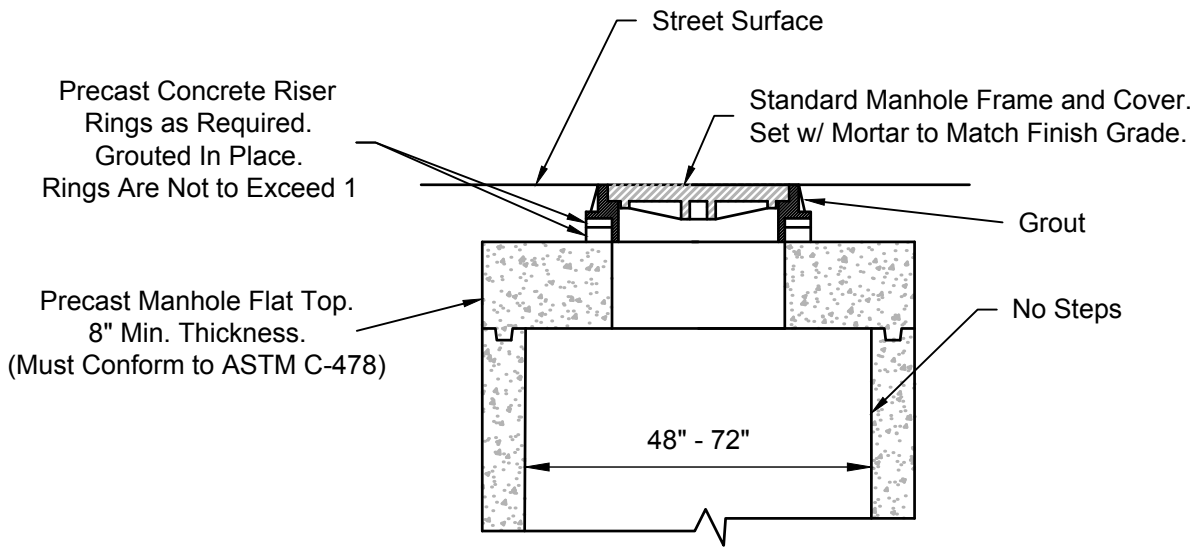
CITY OF DALLAS OREGON  
STANDARD DRAWING AMENDMENT  
STANDARD STORM SEWER MANHOLE

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| 0   | Adoption of Standard Drawings | CTG | 4/11/2016 |

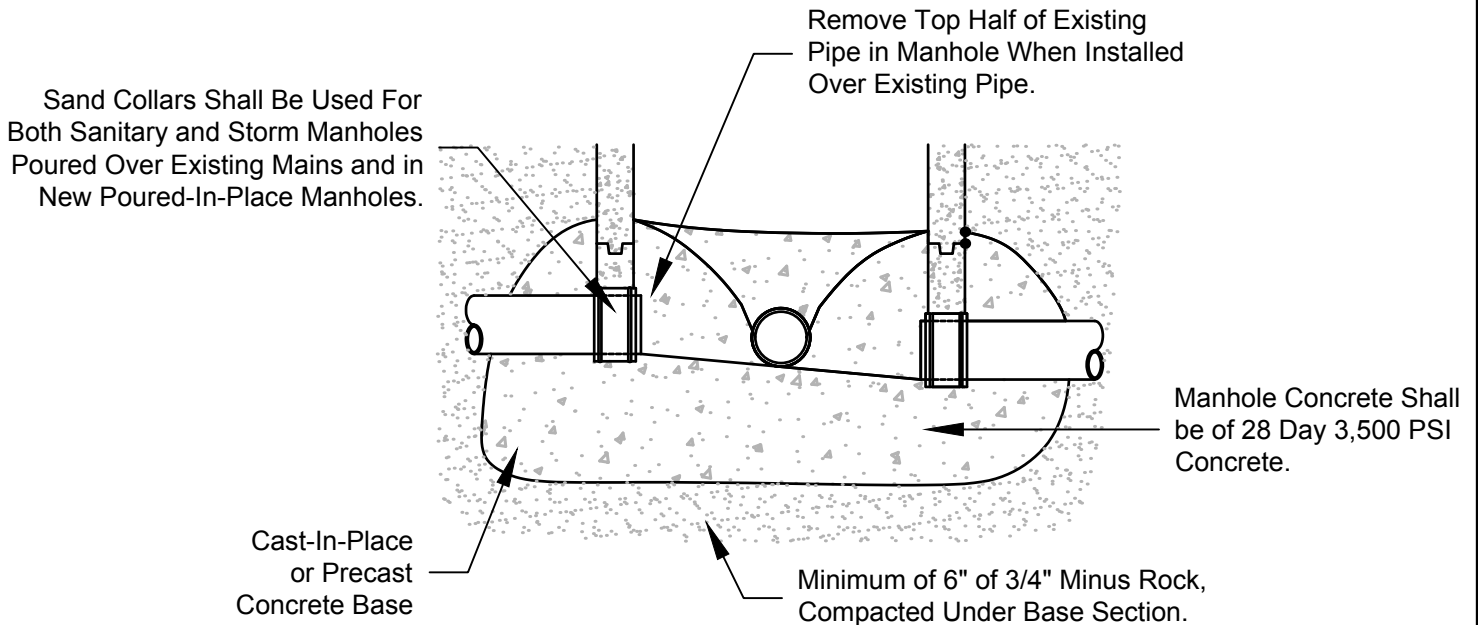


DRAWING NO.  
**RD335**  
**(A)**





**FLAT TOP**



**POURED-IN-PLACE**

**NOTES:**

1. FLAT-TOP MANHOLES SHALL BE CONSTRUCTED WHERE INSIDE HEIGHT IS LESS THAN 4'-0".
2. 28-DAY COMPRESSIVE STRENGTH OF CONCRETE SHALL BE A MINIMUM OF 3,500 PSI.
3. POURED-IN-PLACE MANHOLE BASES SHALL NOT BE CONSTRUCTED WITHOUT PRIOR APPROVAL OF CITY ENGINEER.
4. SEE DETAIL RD335(A) FOR ANY FURTHER APPLICABLE REQUIREMENTS.
5. ALL SANITARY MANHOLES ARE TO PASS A VACUUM OR HYDROSTATIC TEST, SCHEDULED BY CONTRACTOR.

**MODIFICATIONS TO RD342:**

1. DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD342.
2. FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD342.

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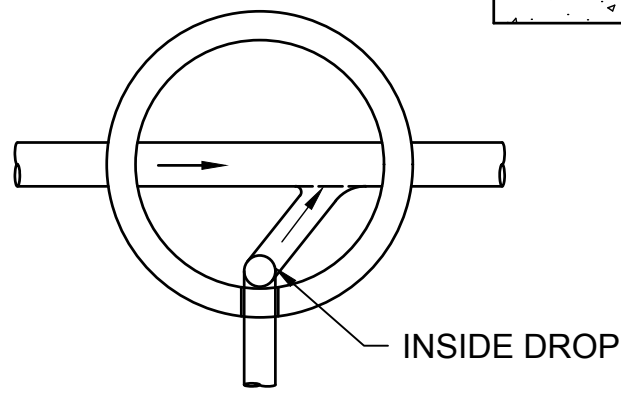
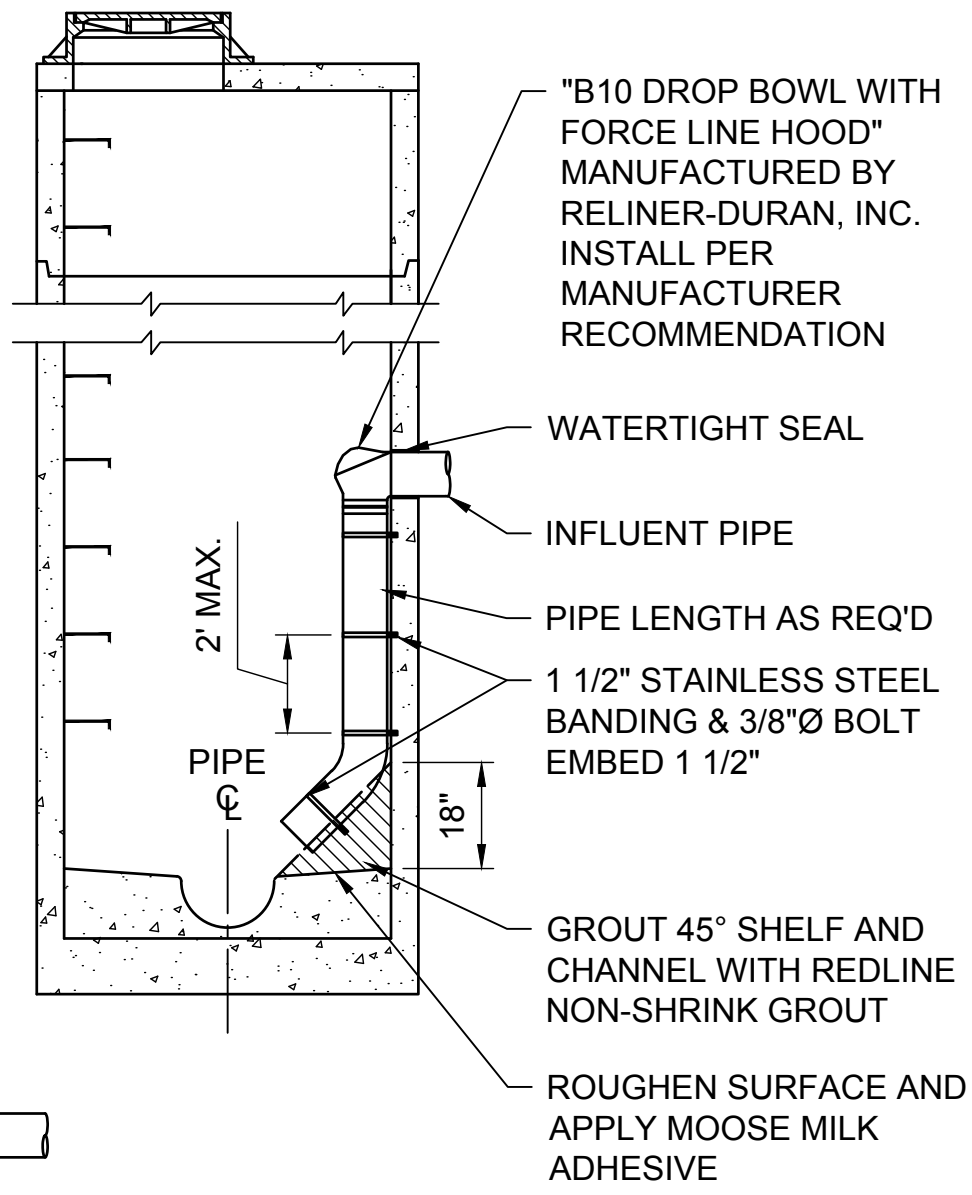


CITY OF DALLAS OREGON  
STANDARD DRAWING AMENDMENT

SHALLOW MANHOLES

DRAWING NO.  
**RD342**  
**(A)**

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**CHANNEL DETAIL**

**NOTES:**

1. ONLY ONE INSIDE DROP CONNECTION ALLOWED PER MANHOLE.
2. MINIMUM MANHOLE DIAMETER WITH DROP CONNECTION SHALL BE 60-INCHES.
3. MAXIMUM DROP PIPE DIAMETER SHALL BE 8-INCHES.
4. DROP MANHOLE ASSEMBLIES SHALL BE CONSTRUCTED WHERE DISTANCE BETWEEN INVERTS EXCEEDS 24".

**MODIFICATIONS TO RD350:**

1. DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD350.
2. FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD350.

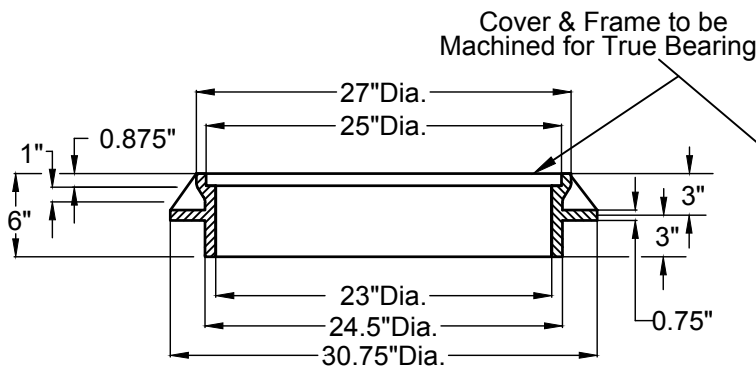
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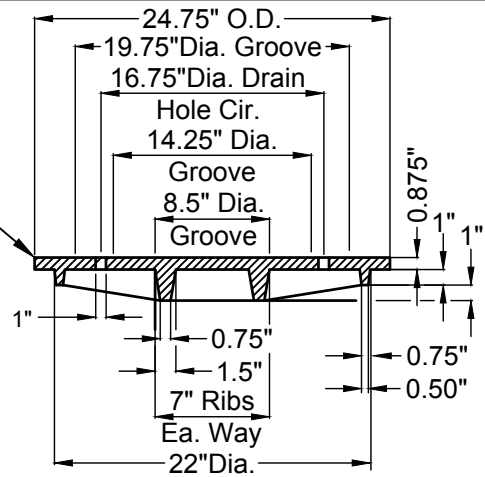
CITY OF DALLAS OREGON  
**STANDARD DRAWING AMENDMENT**  
**STANDARD DROP MANHOLE DETAIL**

DRAWING NO.  
**RD350**  
**(A)**



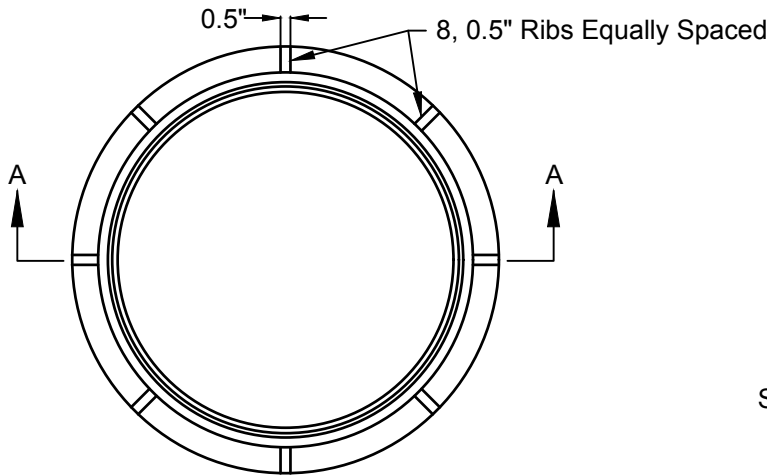


**SECTION A-A**

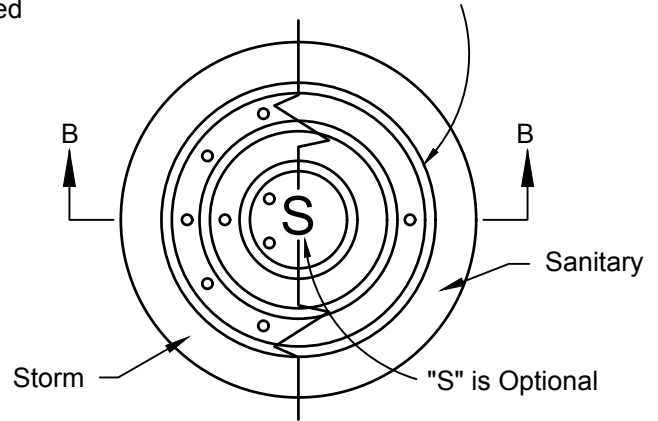


**SECTION B-B**

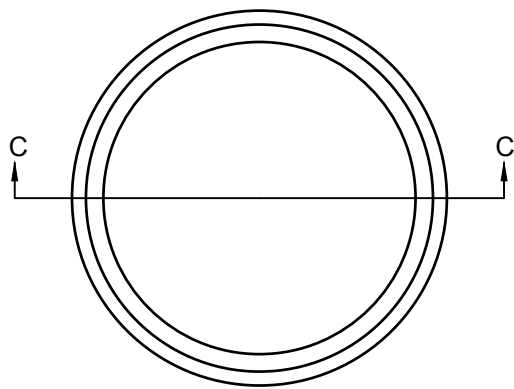
Grooves-Width 1", Depth 0.25"



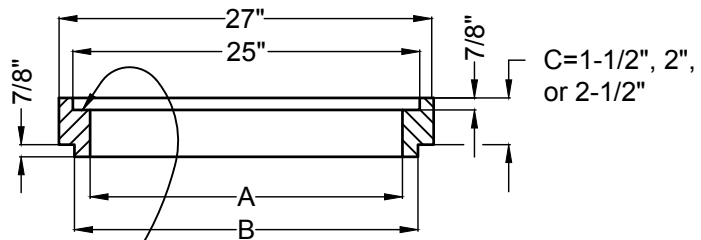
**MANHOLE FRAME**



**MANHOLE COVER**



**ADAPTOR RING**



**SECTION C-C**

| ADAPTOR RING SIZES |         |
|--------------------|---------|
| 24"                | 25"     |
| A 22-1/2"          | 23"     |
| B 23-3/4"          | 24-3/4" |

**NOTES:**

1. MATERIALS: ASTM A-48B CLASS 30 CAST IRON.
2. SANITARY MANHOLES SHALL HAVE TWO 1" DIAMETER DRAIN HOLES.
3. STORM MANHOLES SHALL HAVE SIXTEEN 1" DIAMETER DRAIN HOLES.

**MODIFICATIONS TO RD356:**

1. DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD356.
2. FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD356.

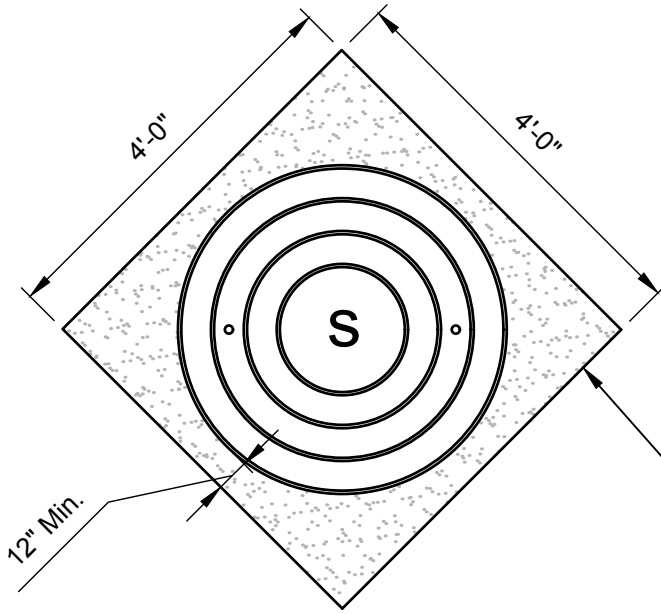


CITY OF DALLAS OREGON  
STANDARD DRAWING AMENDMENT  
MANHOLE COVERS AND FRAMES

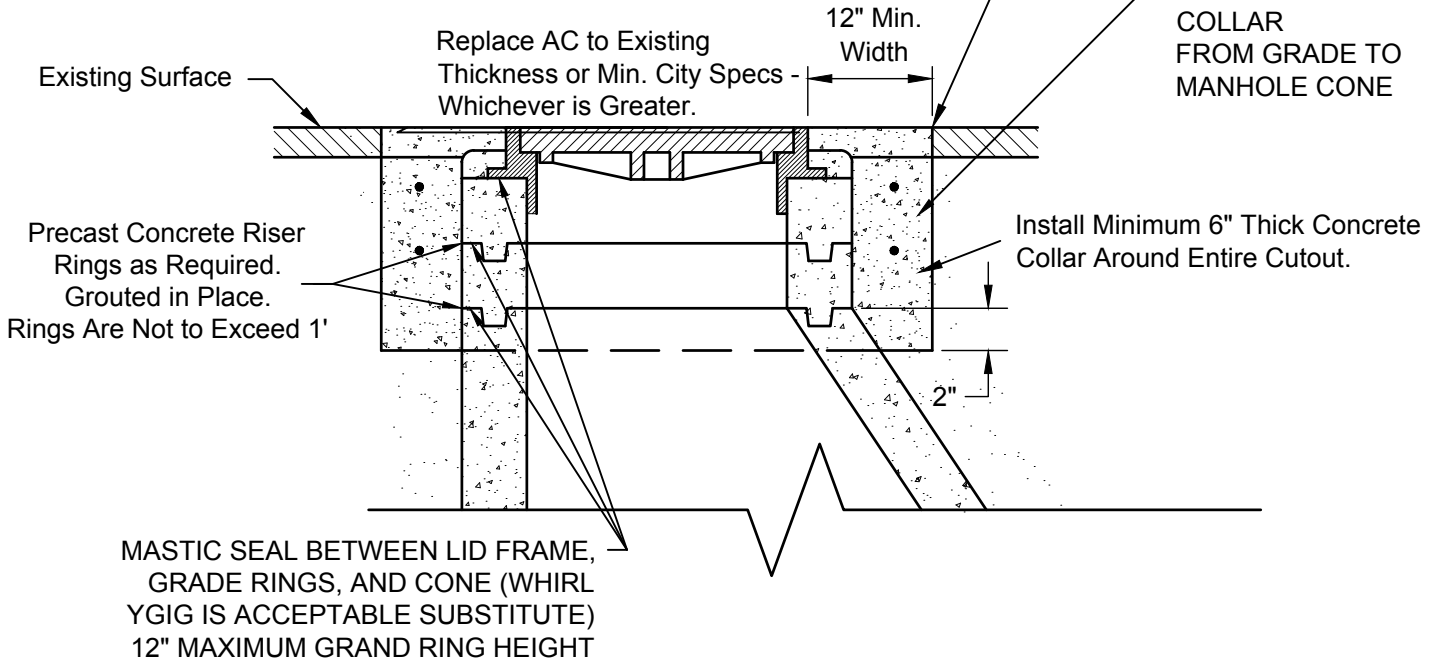
DRAWING NO.  
RD356  
(A)

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Traffic Flow →



Apply Tack Coat to Edges of Existing Pavement. Seal Joint w/ Tack Material and Sand Over.



**MODIFICATIONS TO RD360:**

1. DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD360.
2. FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD360.

**NOTES:**

1. 28-DAY COMPRESSIVE STRENGTH OF CONCRETE SHALL BE A MINIMUM OF 3,500 PSI.
2. SAW CUT DIAMOND AND REMOVE PAVEMENT AS SHOWN. RIMS SHALL BE ADJUSTED TO FINAL GRADE PRIOR TO PAVING.

|     |                               |     |           |
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CITY OF DALLAS OREGON  
 STANDARD DRAWING AMENDMENT  
 STANDARD MANHOLE  
 ADJUSTMENT IN STREETS

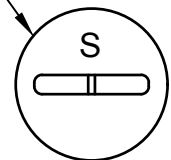
DRAWING NO.  
**RD360**  
**(A)**

Tracer wire, leave enough free wire to extend 18" above cast iron cover

See note 3

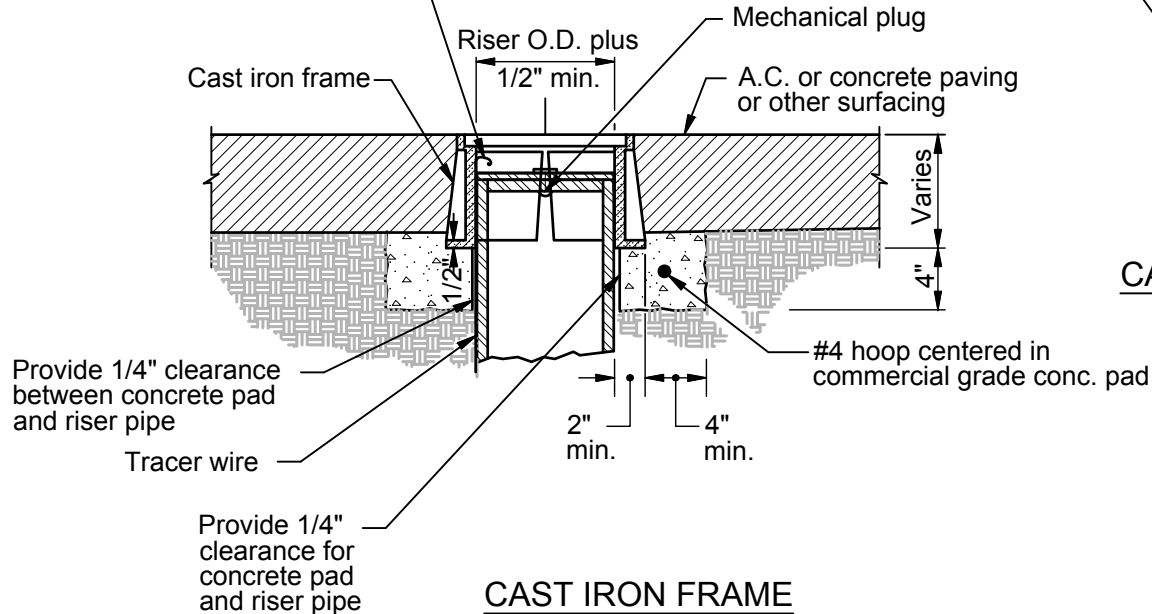


Profile View

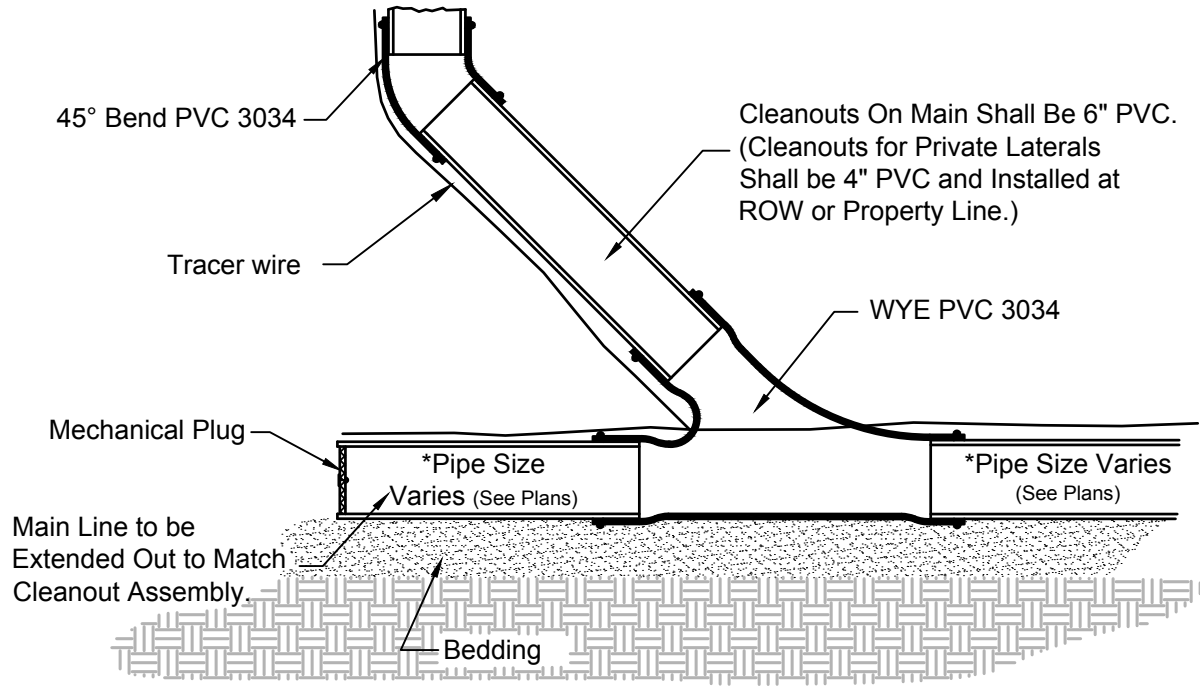


Plan View

**CAST IRON COVER**



**CAST IRON FRAME**



**NOTES:**

1. ALL CLEANOUT MATERIAL TO BE SAME AS CARRIER PIPE.
2. BROOKS BOXES CAN BE USED IN NON-PAVED AREAS.
3. CAST IRON LID SHALL READ "S", "SEWER", or "CO".
4. PLACE GREEN 2x4 AT END OF MAIN LINE IF SEWER IS TO BE EXTENDED FOR FUTURE PHASES.
5. FUTURE PIPE DOES NOT NEED TO BE INSTALLED AT MAIN IF MAIN IS NOT GOING TO BE INSTALLED IN THE FUTURE.

**MODIFICATIONS TO RD362:**

1. DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD362.
2. FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD362.

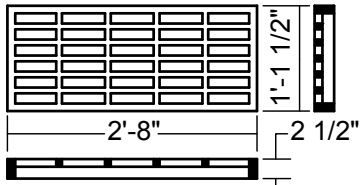
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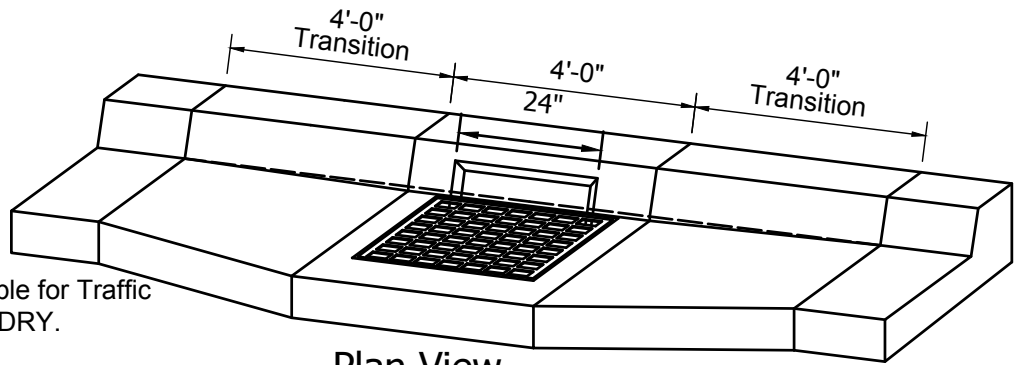


CITY OF DALLAS OREGON  
STANDARD DRAWING AMENDMENT  
STANDARD SANITARY-STORM  
CLEANOUT DETAIL

DRAWING NO.  
**RD362**  
**(A)**



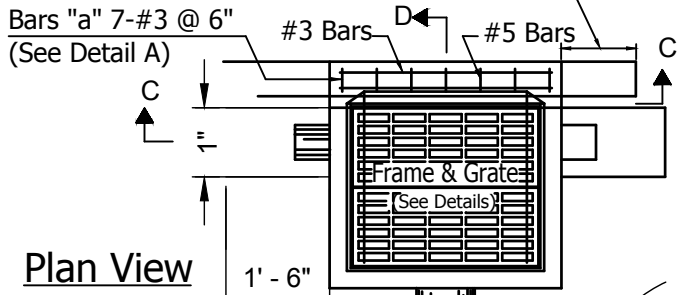
**GRATE**



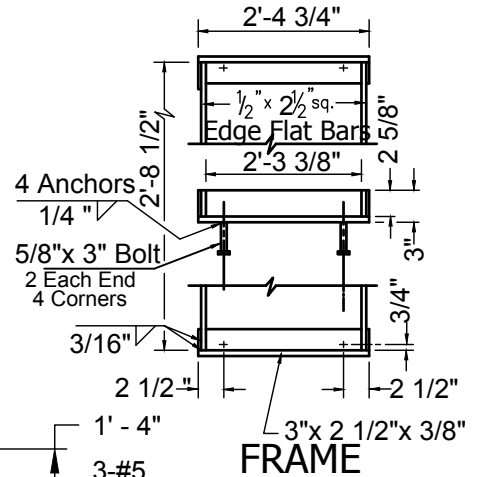
**Plan View**

- 1.) Grates Shall be Ductile Iron Suitable for Traffic Loading as MFD. BY INLAND FOUNDRY.
- 2.) 2 Grates Required for Each C.B.

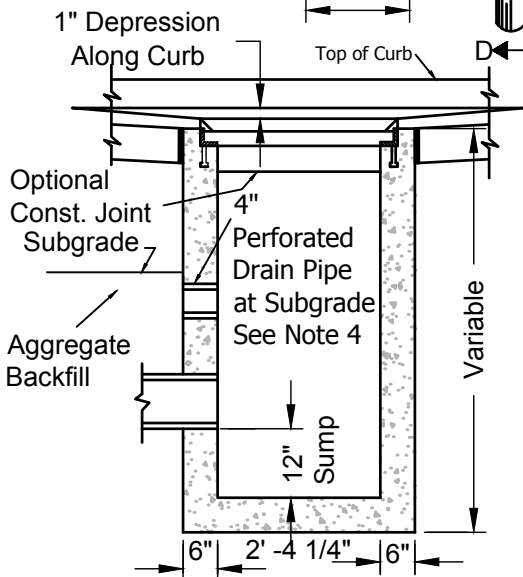
Sawcut, Remove & Replace Exist. Curb 2' Min. From C.B. Wall (Typ. Both Sides)



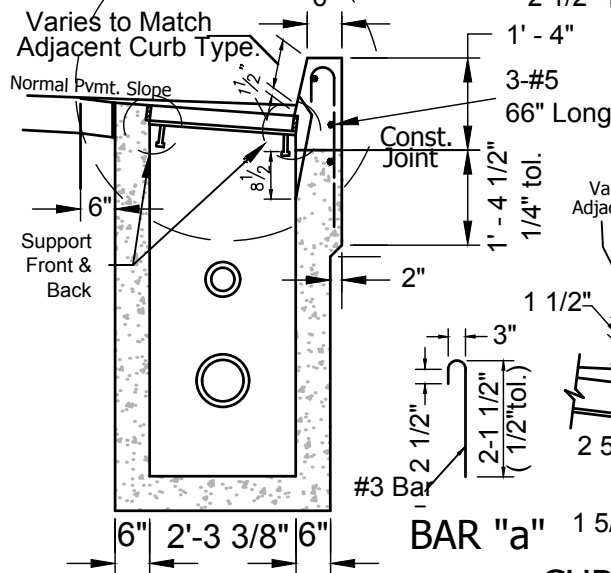
**Plan View**



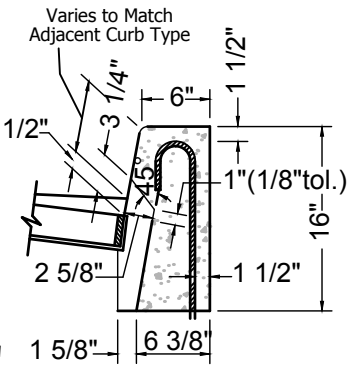
**FRAME**



**SECTION C-C**



**SECTION D-D**



**CURB OPENING DETAIL A**

**NOTES:**

1. REINFORCING STEEL:  
A.) NO. 3 BARS TO BE PLACED DURING CURB CONSTRUCTION.  
B.) ALL BARS TO BE PLACED 1/2" CLEAR OF NEAREST FACE OF CONCRETE UNLESS SHOWN OR NOTED OTHERWISE.  
C.) ALL BAR SPLICES SHALL BE 20 DIA.
2. ALL CONCRETE SHALL HAVE COMP STRENGTH OF 3,500 PSI.
3. MATERIALS FOR FRAMES AND GRATES SHALL CONFORM TO SEC. 02450 OF STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION BY OREGON STATE HIGHWAY DIVISION, 1991.
4. 4" SUB-GRADE DRAIN MUST BE PLACED ON UPHILL SIDE OF CATCHBASIN.
5. ALL CURBED CATCH BASINS MUST HAVE RE-BAR.

**MODIFICATIONS TO RD366:**

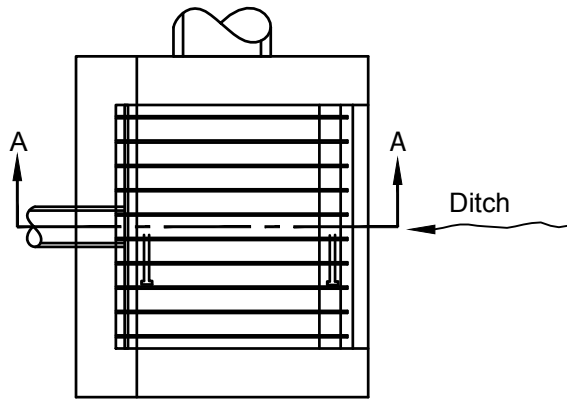
1. DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD366.
2. FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD366.



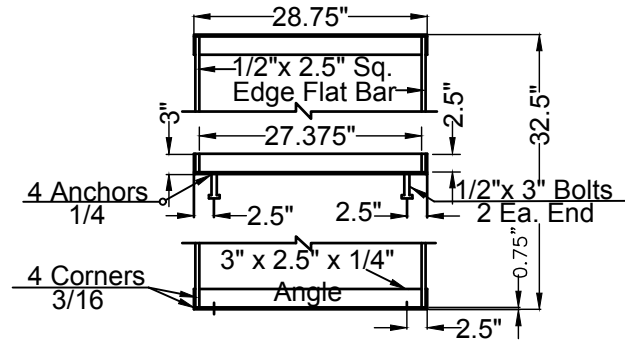
CITY OF DALLAS OREGON  
STANDARD DRAWING AMENDMENT  
STANDARD CATCH BASIN DETAIL (CG-2)

DRAWING NO.  
**RD366**  
**(A)**

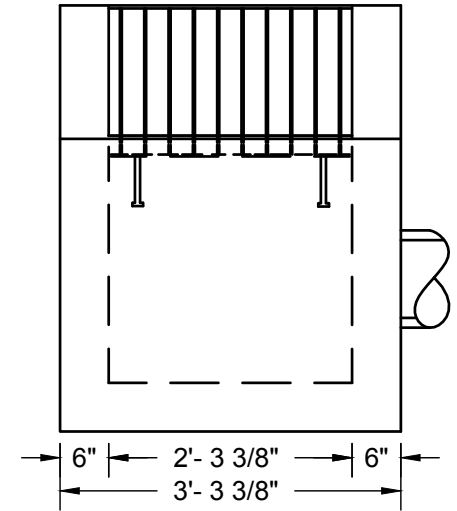
|     |                               |     |           |
|-----|-------------------------------|-----|-----------|
| 0   | Adoption of Standard Drawings | CTG | 4/11/2016 |
| NO. | REVISIONS                     | BY  | DATE      |



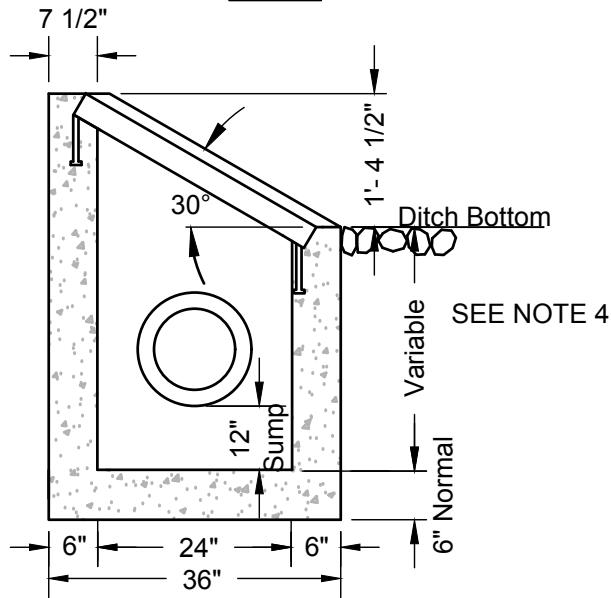
**PLAN**



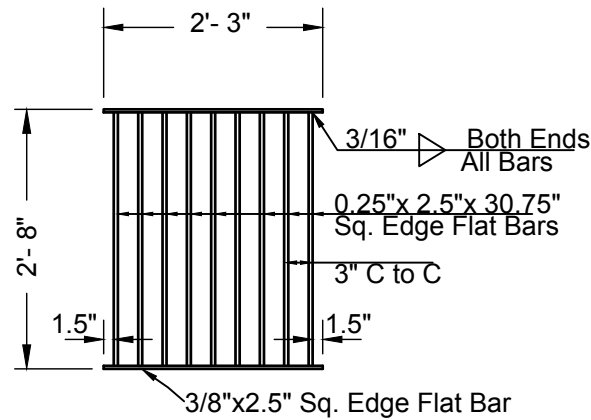
**FRAME**



**ELEVATION**



**SECTION A-A**



**GRATE**

**MODIFICATIONS TO RD370:**

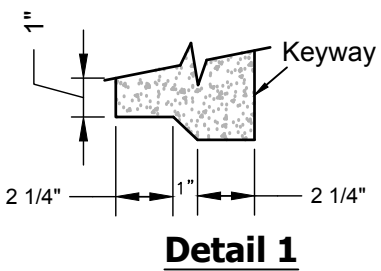
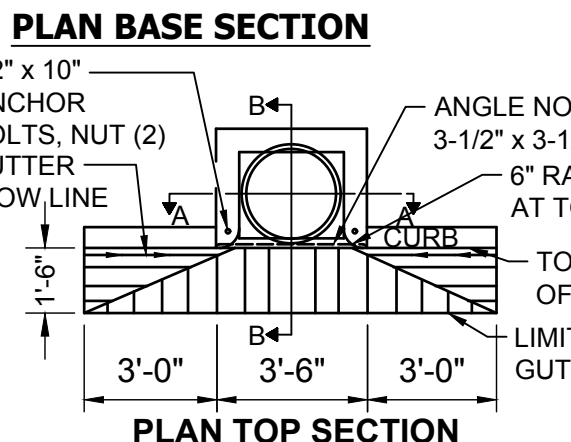
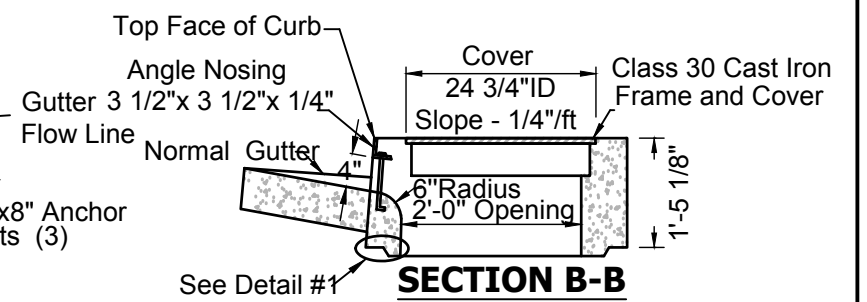
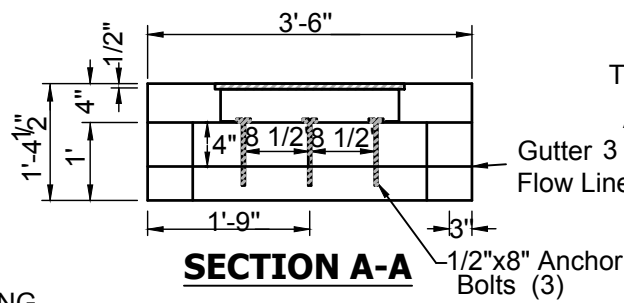
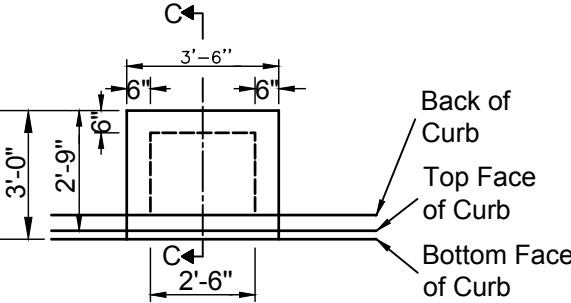
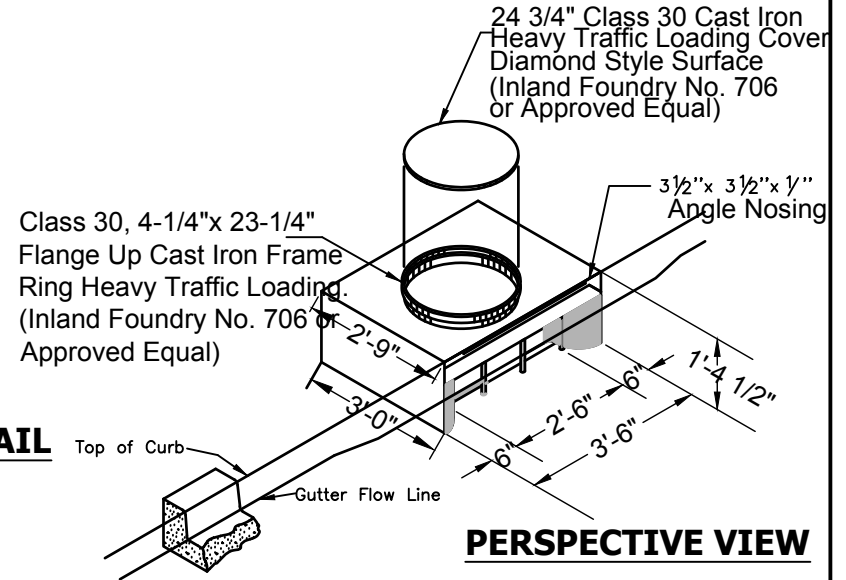
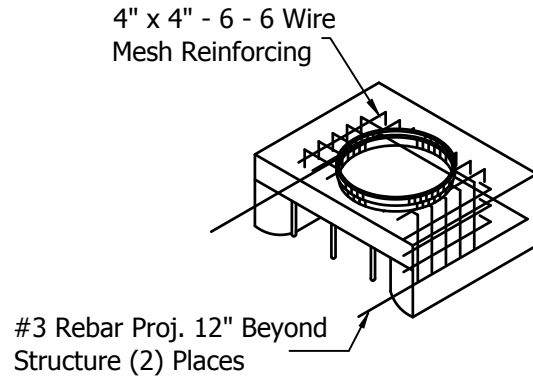
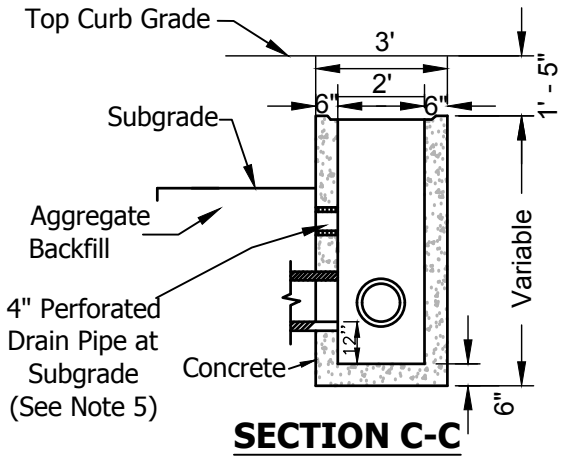
1. DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD370.
2. FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD370.

**NOTE:**

1. CONCRETE SHALL HAVE A COMP. STRENGTH OF 3500 P.S.I.
2. FOR OUTLET SIZE, LOCATION AND INVERT ELEVATION, SEE PROJECT PLANS.
3. FRAME AND GRATE MATERIAL SHALL BE STEEL (A.S.T.M. A-36) AND GALVANIZED IN ACCORDANCE WITH (A.S.T.M. A-123).
4. PLACE CLASS 50 RIP RAP IN FRONT OF CATCHBASIN. 4'-5' LONG 1' DEPTH.

|   |                               |     |           |
|---|-------------------------------|-----|-----------|
| <b>CITY OF DALLAS OREGON<br/>STANDARD DRAWING</b> |                               |     |           |
| <b>FIELD INLET (TYPE D)</b>                       |                               |     |           |
|   |                               |     |           |
| 0   | Adoption of Standard Drawings | CTG | 4/11/2016 |
| NO.   | REVISIONS                     | BY  | DATE      |

DRAWING NO.  
**RD370**  
**(A)**



- MODIFICATIONS TO RD371:**
1. DETAILS, DIMENSIONS AND SPECIFICATIONS SHOWN HERE AMEND THE DETAILS SHOWN ON OREGON STANDARD DRAWING RD371.
  2. FOR ADDITIONAL DETAILS AND DETAILS NOT SHOWN HERE, SEE OREGON STANDARD DRAWING RD371.

- NOTE:**
1. CONCRETE SHALL HAVE A COMP. STRENGTH OF 3500 P.S.I.
  2. TOP SHALL BE REINFORCED WITH 4"X4"-6-6 WIRE MESH.
  3. ALL METAL PARTS SHALL BE HOT DIPPED GALVANIZED AFTER FABRICATION.
  4. COVER SHALL BE ASTM A-48 CLASS 30 CAST IRON.
  5. 4" SUB-GRADE DRAIN MUST BE PLACED ON UPHILL SIDE OF CATCHBASIN.
  6. FOR OUTLET SIZE, LOCATION AND INVERT ELEVATION, SEE PROJECT PLANS.

**CITY OF DALLAS OREGON  
STANDARD DRAWING**

**STANDARD CURB INLET AND BASE DETAIL (CG-3)**

|     |                               |     |           |
|-----|-------------------------------|-----|-----------|
|     |                               |     |           |
|     |                               |     |           |
|     |                               |     |           |
| 0   | Adoption of Standard Drawings | CTG | 4/11/2016 |
| NO. | REVISIONS                     | BY  | DATE      |

DRAWING NO.  
**RD371**  
**(A)**